

FACT SHEET

Public Meeting | February 5, 2019

E. BROAD ST. IMPROVEMENTS

I-270 to Outerbelt St.

Project Overview

The City of Columbus is proposing streetscape improvements along East Broad St. from just east of I-270 to Outerbelt St. to improve congestion and safety. Proposed enhancements include pedestrian and bicycle facilities, new through and turn lanes, curbed median, street lighting and signage.

Project Goals



Improve Corridor Traffic Flow

The E. Broad St. corridor is currently operating at capacity. This project will add both through and turn lanes throughout the corridor in order to relieve current congestion as well as plan for increased future traffic.



Reduce Crash Rates

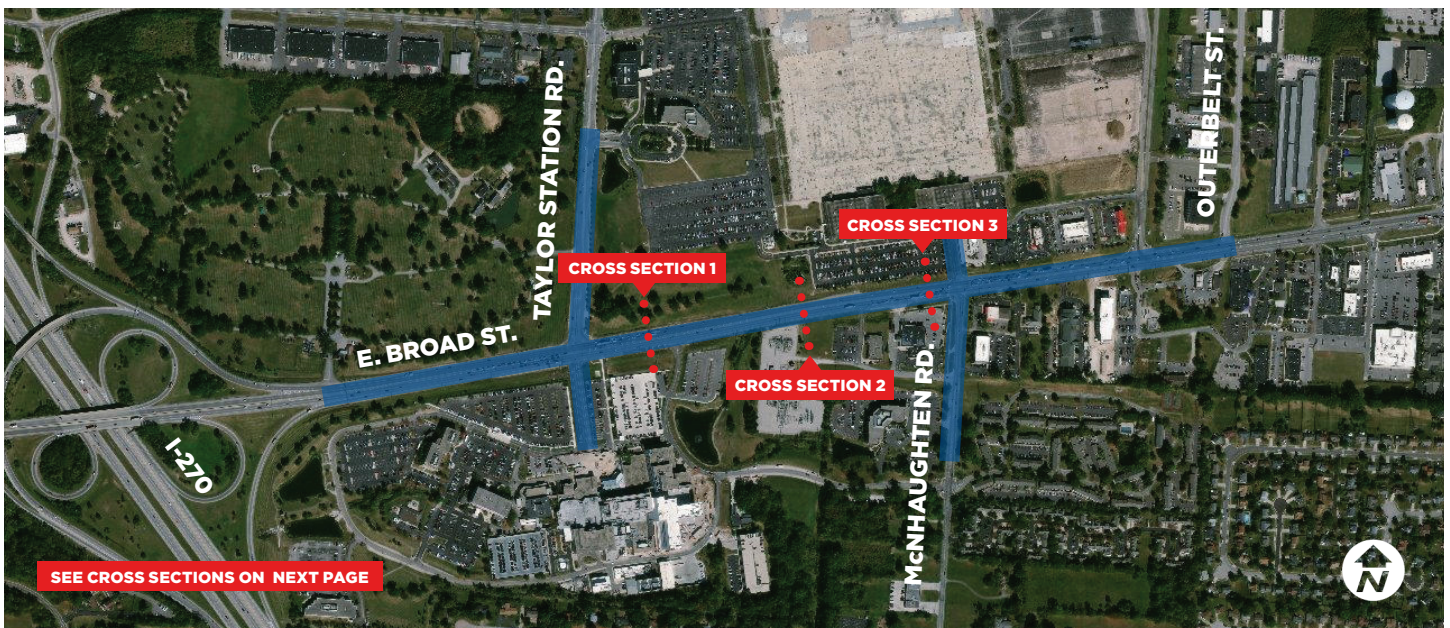
E. Broad St./McNaughten and Broad St./Taylor Station Rd. rank #27 and #80, respectively, on the Mid-Ohio Regional Planning Commission's (MORPC) Top 100 High-Crash locations list (2014-2016). Crash rates are expected to be reduced by installing a center median along East Broad St., which will eliminate turning conflicts at driveways and intersections, and the new roadway lanes, which will reduce rear end collisions.



Increase Safety for Pedestrians and Bicyclists

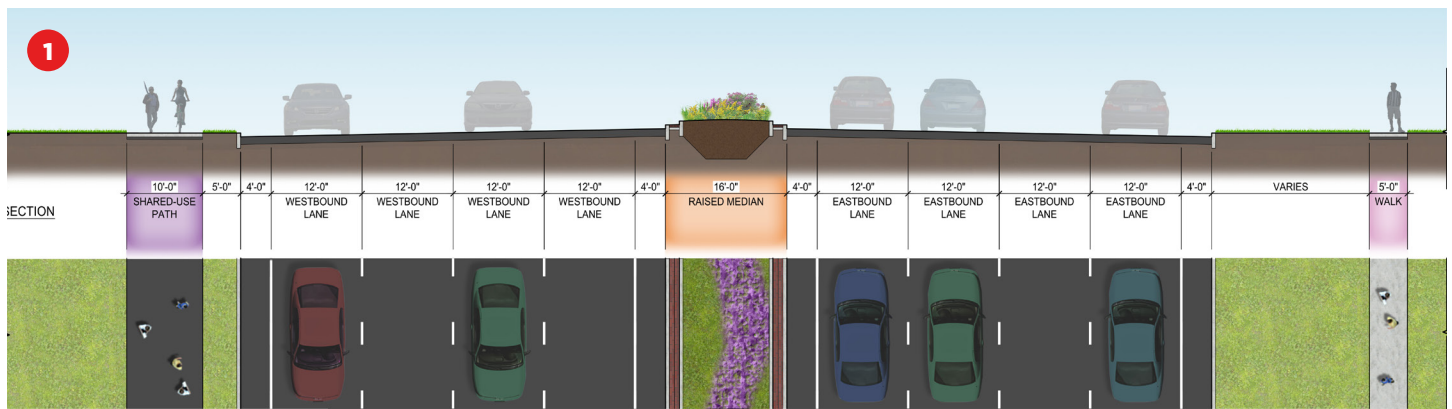
There are currently limited facilities for safe walking and bicycling. New sidewalks, curb ramps, clearly marked crossings with pedestrian signals at signalized intersections and the construction of a shared use path along the north side of East Broad Street will make it safer and more attractive for pedestrians and bicyclists.

Project Area - Phase 1*



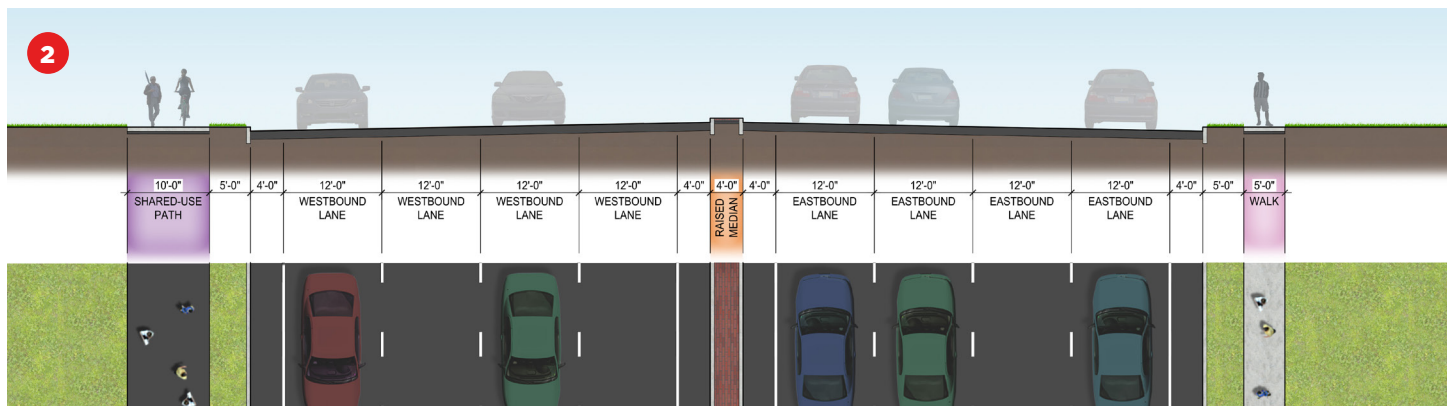
*Future phases dependent on funding availability.

Cross Sections



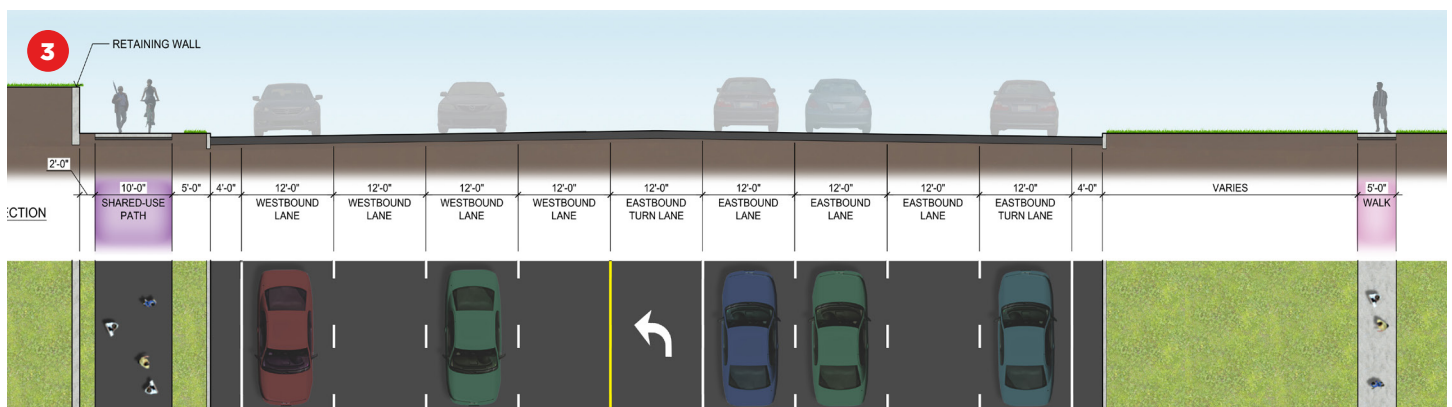
Looking east along E. Broad St. near Ganse Ln.

New 10' wide shared-use asphalt path / 4 westbound lanes / 16' wide raised enhanced center median / 4 eastbound lanes / new 5' wide concrete sidewalk



Looking east along E. Broad St. near the Columbus & Southern electrical substation

New 10' wide shared-use asphalt path / 4 westbound lanes / 4' wide raised center median / 4 eastbound lanes / new 5' wide concrete sidewalk

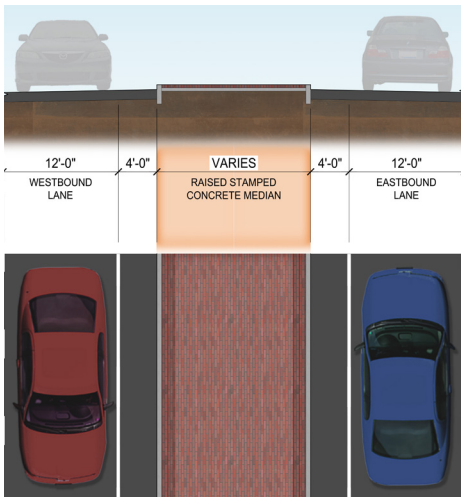


Looking east along E. Broad St. near McNaughten Rd.

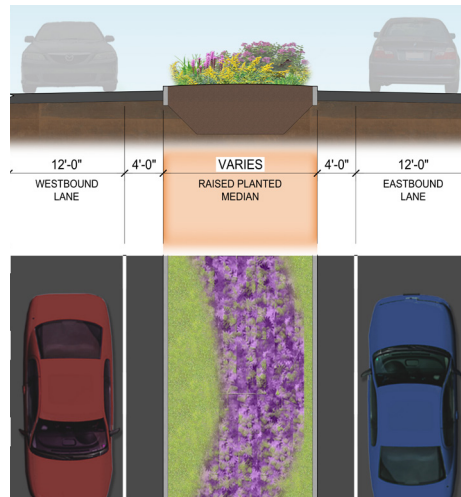
New retaining wall / new 10' wide shared-use asphalt path / 4 westbound lanes / 1 eastbound left turn lane / 4 eastbound lanes / new 5' wide concrete sidewalk

Center Median Options

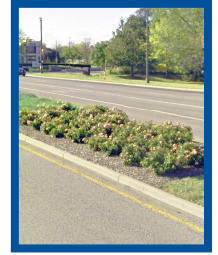
Two median options are proposed along the corridor.



Option 1 – Raised median with stamped concrete



Option 2 – Raised median with ground level plantings



Other Features



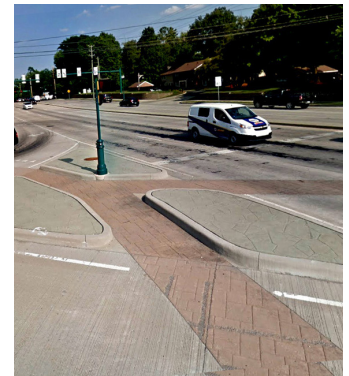
Shared Use Path – New 10' wide asphalt path along corridor for pedestrian and bicyclists



Concrete Sidewalk – New 5' wide concrete path along corridor for pedestrians



Curb Ramps – New curb ramps to meet ADA standards



Pedestrian Safety Island – New island to enhance safety for bicyclist and pedestrian crossings



Crosswalks – New crosswalks to improve pedestrian safety



Drive Apron – New drive aprons and curb cuts along corridor for more defined vehicle access

Project Costs

Study	\$400,000
Design	\$1.1M
Right-of-way Acquisition	\$1.1M*
Utility Relocations	\$400,000*
Construction	\$13.8M*
TOTAL	\$16.8M

*60% funded by Mid-Ohio Regional Planning Commission.

SCHEDULE

Environmental Overview

2017–Spring 2020

Design

2017–Summer 2020

Right-of-Way Acquisition

Summer 2020–January 2022

Construction

Summer 2022–Late Fall, 2023

Stakeholder/Public Engagement

Ongoing

CONTACT

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columbus.gov/Templates/Detail.aspx?id=2147505081