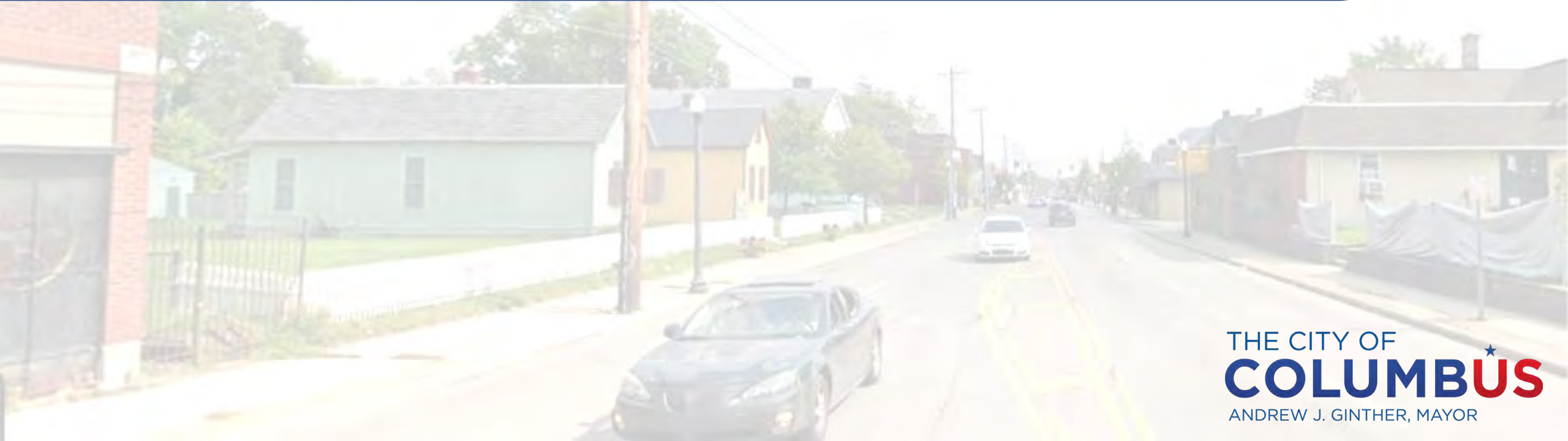


Livingston Avenue Improvements Study 18th Street to Nelson Road

Public Meeting #2

March 23, 2023



Meet the Team

City of Columbus:

Dept. of Public Service

- Design and Construction
- Planning
- Public Affairs
- Traffic Management

Dept. of Development

Dept. of Neighborhoods

Steering Committee

CHA (Engineering and Design)

Lawhon & Associates (Public Involvement)



Steering Committee

Michael Aaron

Rickenbacker Woods Community Fdn.

Pastor Jonathan Bradford

Bethany Baptist Church

Danielle Dillard

Livingston Avenue Area Comm.

David Gray

Livingston Avenue Area Comm.

Stacee Green

Driving Park Civic Assoc.

Rita Hallaveld

Resident & cyclist

Molly Hogue

Resident

George Holliman

Livingston Avenue Area Comm.

Rachel Robinson

Southern Orchards Civic Assoc.

Jerry Saunders

Africentric Personal Dev. Shop

Erin Synk

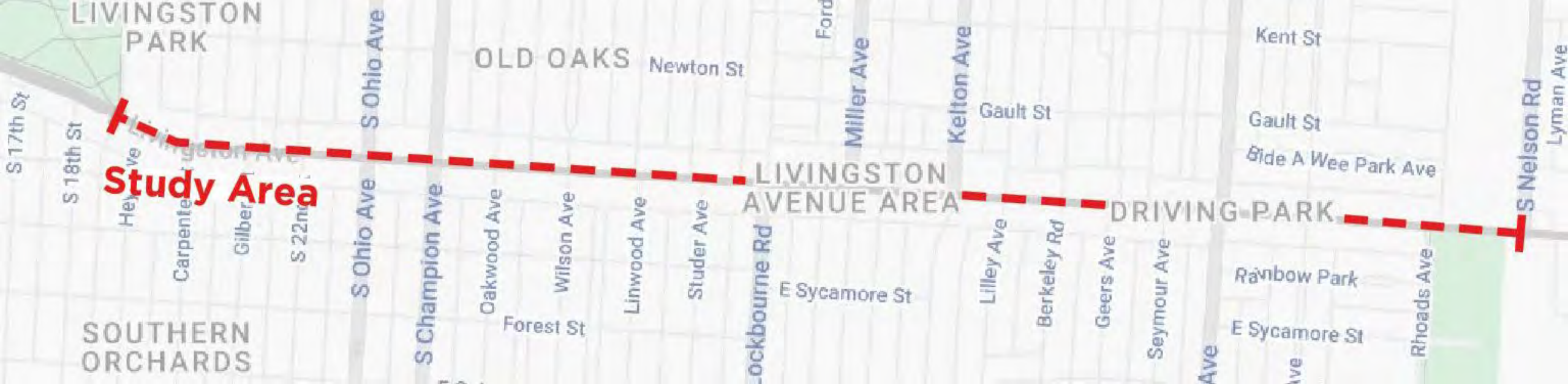
Columbus South Side Area Comm.

Ralph Walker

Livingston Ave. Columbus, OH Safety Group

Sherman Willis

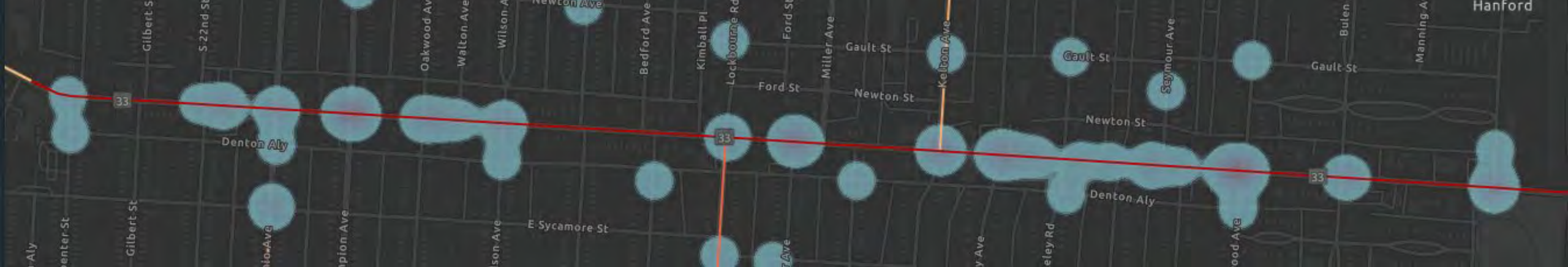
Community business owner



Study Limits

- Study limits extend along Livingston Avenue from S. 18th Street to Nelson Road
- This section of Livingston Avenue is also designated as a part of U.S. Rt. 33





Why?

Vision Zero

- Project corridor has **6x** the fatality rate, and **5x** the serious injury rate than the Columbus Metro area.
- The corridor is part of the city and regional High Injury Networks.
- Speeding and safety concerns significantly limit opportunities for residents and businesses.



<https://vision-zero-columbus.hub.arcgis.com/>

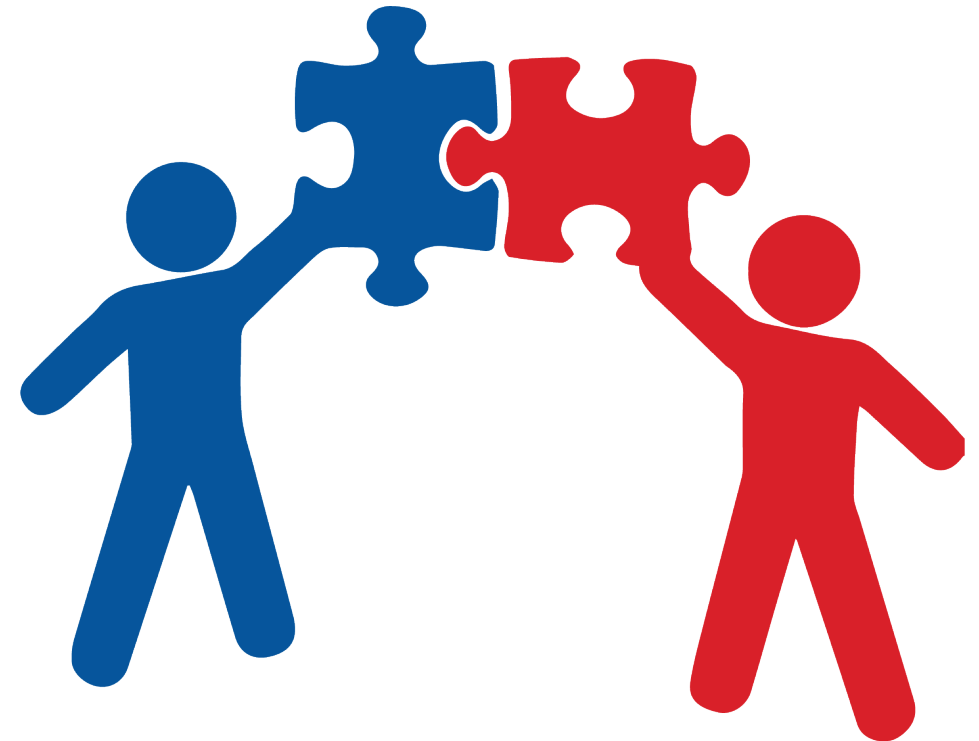
Desired Outcomes

Study

- Work **with** the Community to develop clear design objectives
- Perform necessary traffic analyses to develop effective countermeasures to speeding and safety concerns

Project as a whole

- Address safety concerns for all modes of transportation
- Improve access to businesses and other community assets throughout the corridor and beyond
- Celebrate the community and its rich history



What is the goal for today?

- Discuss the study process
- Present what we've done so far
- Present findings and recommendations from study
- Discuss next steps



Planning Process and Public Engagement

Chantil Milam, Lawhon & Associates

How We Will Use the Study

- Study used to identify issues, conduct analysis, receive and document input, narrow down solutions, provide recommendations
- Study then used as a roadmap/reference for future project(s)



Public Involvement



Aug. 4
Steering
Committee
Meeting

1



Sep. 13
Steering
Committee
Meeting

2



Oct. 11
Steering
Committee
Meeting

3



November 17
Public
Meeting

1



December 13
Steering
Committee
Meeting

4

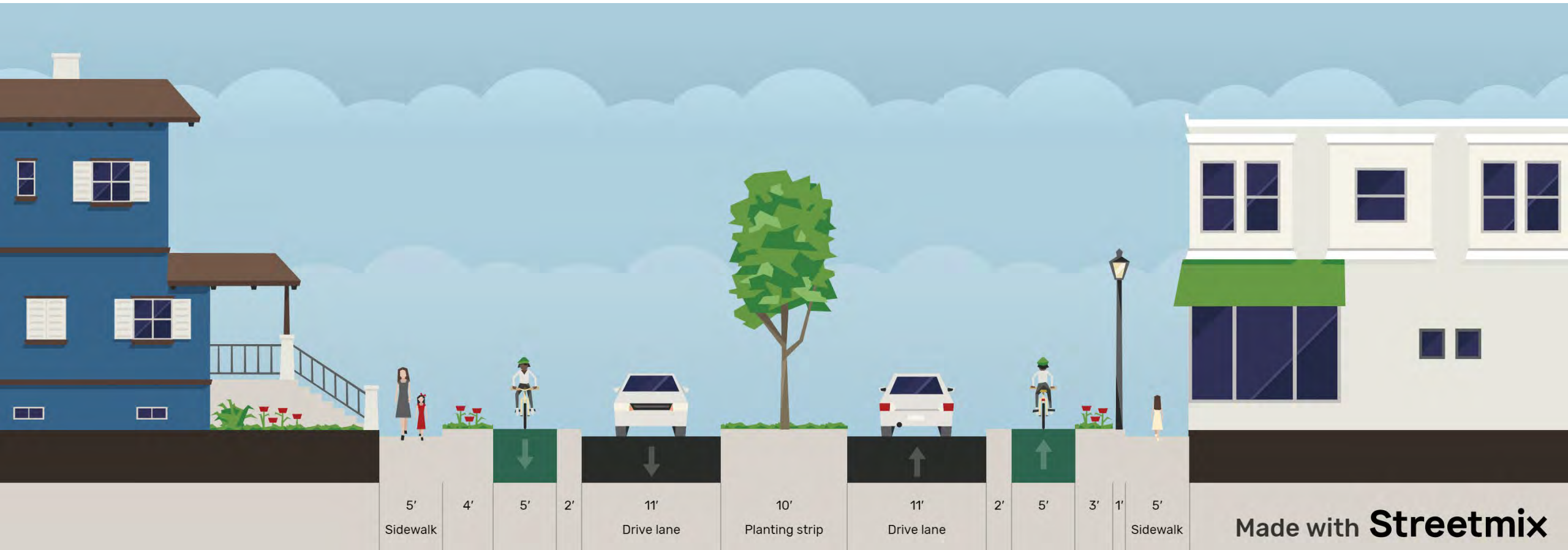


February 21
Steering
Committee
Meeting

5

Planning

Public Meeting #1 – Recommended Concept



Note: This is a conceptual image for illustrative purposes only. Green is being used to illustrate the location of bike lanes. Final pavement markings will be determined in design.

Public Comments

What do you *like* about this preliminary concept/layout for Livingston Avenue?

- Dedicated bike lanes (23)
- Raised bike lanes (16)
- Concept will slow traffic/improve safety (15)
- Reduced lanes of traffic (11)
- Removal of on-street parking (6)

What do you *dislike* about this preliminary concept/layout for Livingston Avenue?

- Medians too long/limited access to side streets and left turns (10)
- Removal of on-street parking (8)
- Reduced lanes of traffic (6)

What We Heard



Improve safety for both pedestrians and motorists



Explore dedicated bike facilities
(local, regional connectivity)



Create opportunities to add character/beauty into
corridor



Balance identified needs with existing infrastructure
(on-street parking, busses)



Livingston Avenue Improvement Study

Cory Lamb, CHA

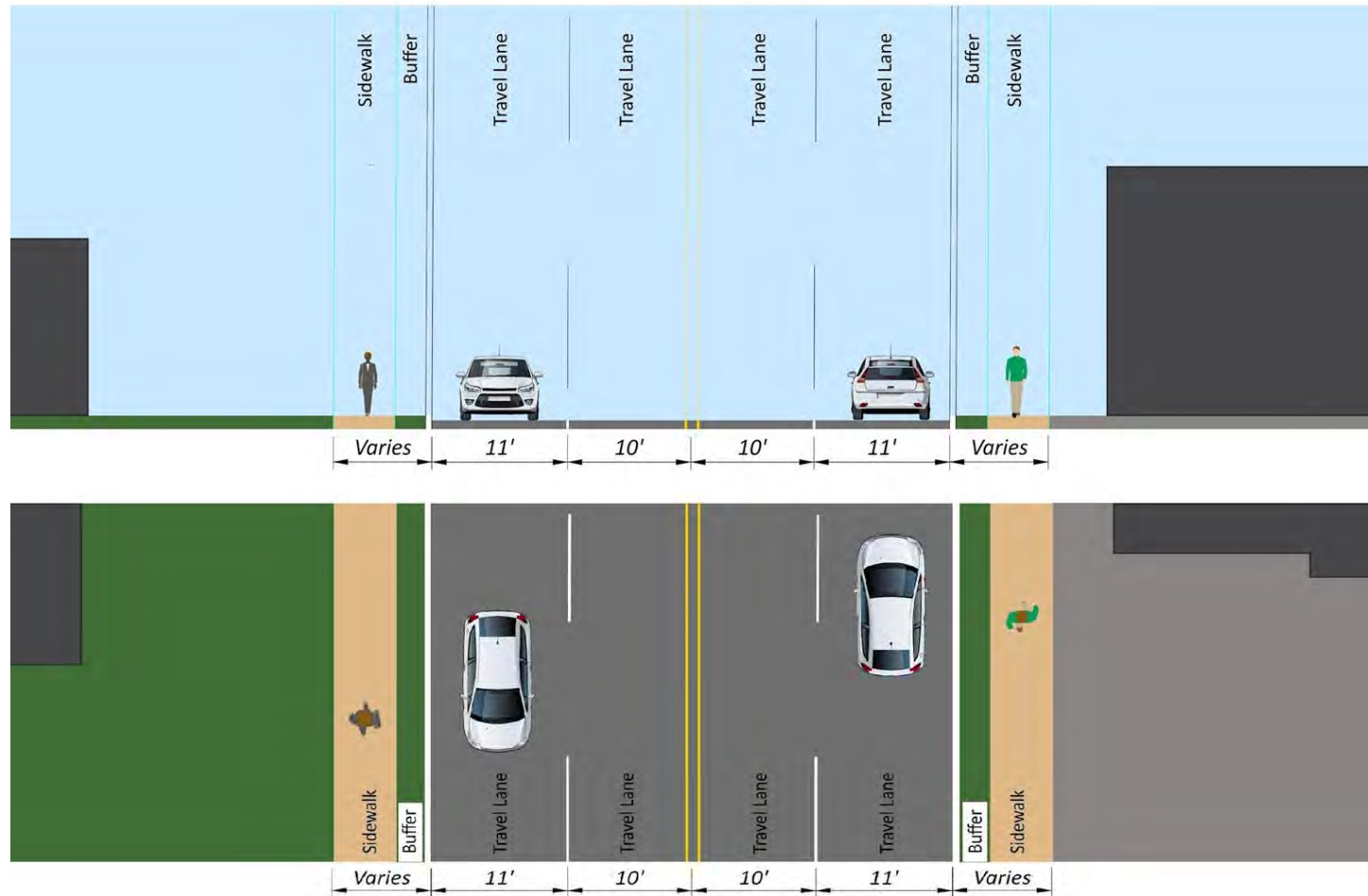
What We've Done

- Existing Conditions
- Traffic Analysis
- Recommendations
- Design Standards



What We've Done - Existing Conditions

Roadway Cross Section



What We've Done – Existing Conditions and Traffic

Pedestrian, Bicycle, Transit and Vehicular Facilities



What We've Done – Recommendations

Short Term Countermeasures

- Improve Signal Visibility
- Signal Retiming
- Prohibit Left Turns
- Pedestrian Crosswalk



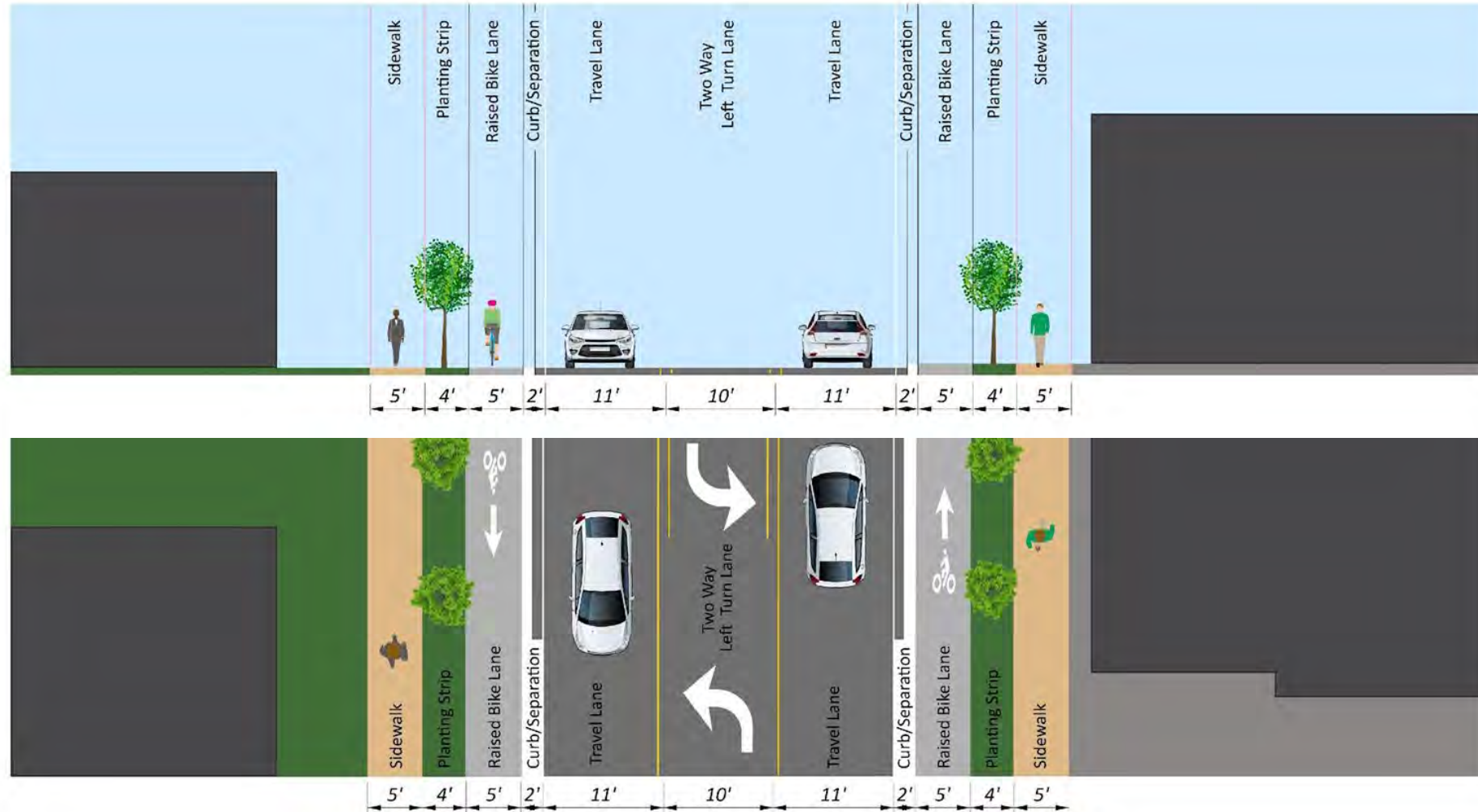
What We've Done – Recommendations

Bicycle Lanes, Pedestrians and Parking



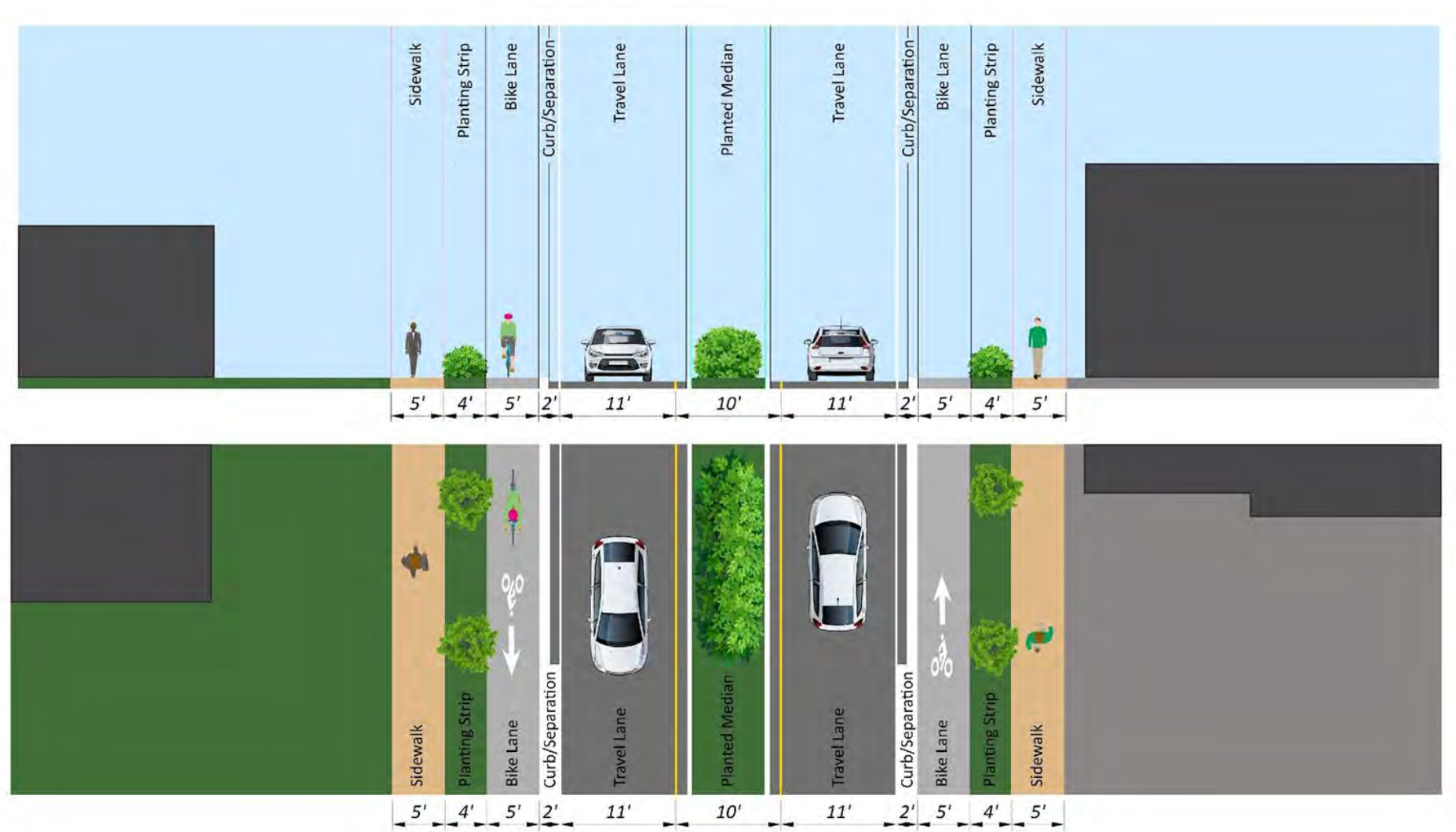
What We've Done – Recommendations

Road Diet with Two-Way Left Turn Lanes



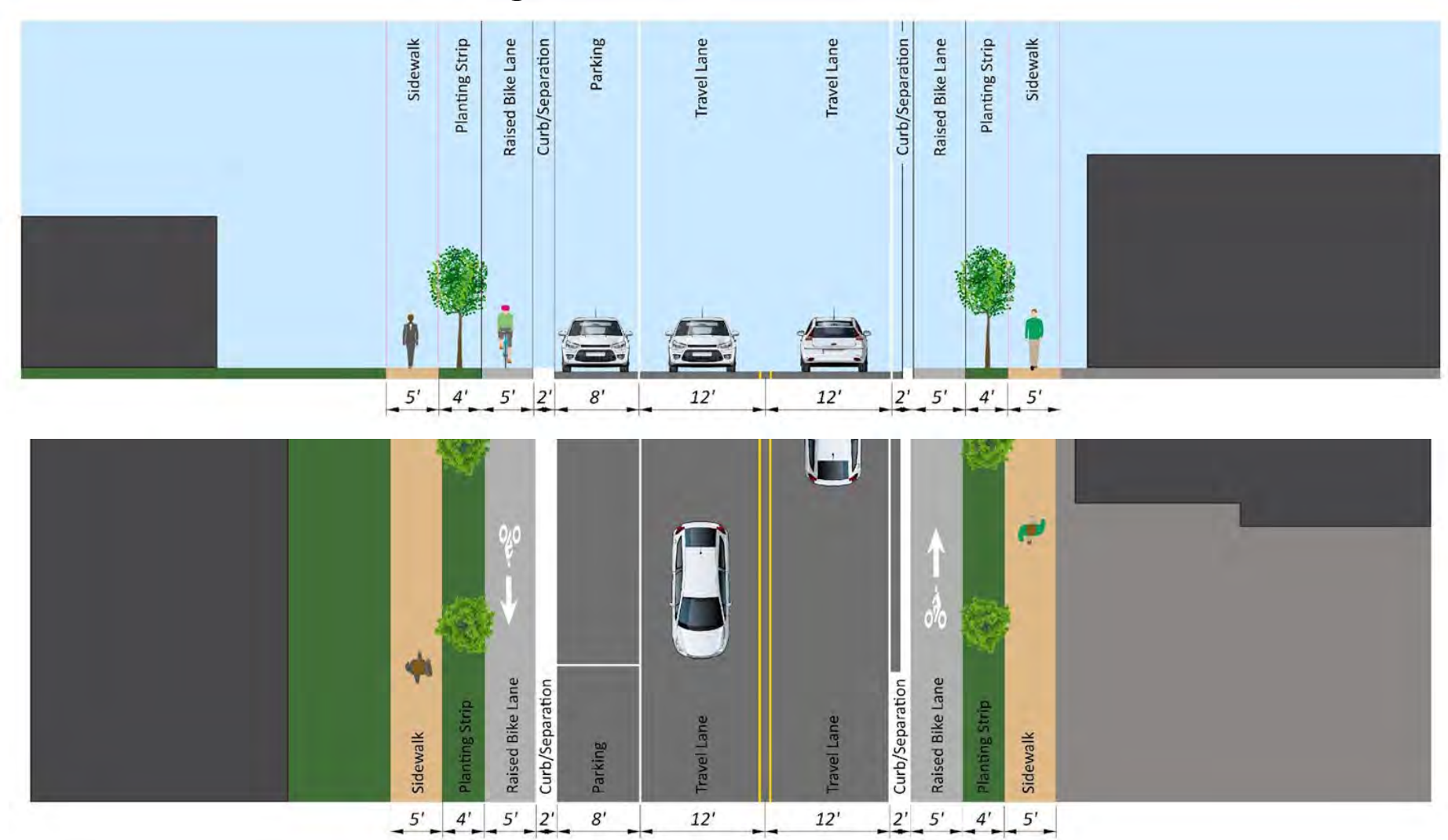
What We've Done – Recommendations

Road Diet with Median



What We've Done – Recommendations

Road Diet with Parking



What We've Done – Recommendations

Project Transitions – Eastern and Western Limit



Western project limit

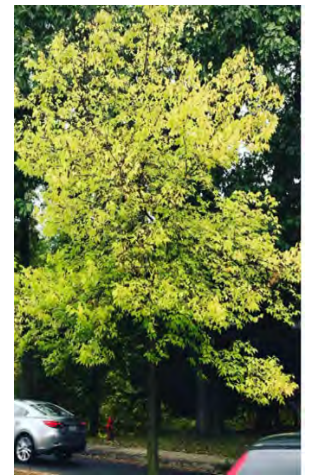


Eastern project limit

What We've Done – Recommendations

Design Standards

- Streetscape Design
- Bicycle Facilities
- Bus Stop Guidance



What We've Done – Recommendations

Design Standards – Bus Stop Guidance



Livingston Avenue Improvements – Commercial Area



Livingston Avenue Improvements – Commercial Area



Livingston Avenue Improvements – Residential Area



Livingston Avenue Improvements – Residential Area



Livingston Avenue Improvements – Residential Area





Next Steps

Eric Lambert, City of Columbus

Study/Project Support

Columbus recently awarded \$12 million for Safe Streets for All (SS4A) Implementation Grant with a local match of \$12 million for a total of \$24 million.

“This investment will significantly improve the safety and accessibility of the corridor, businesses and other community assets. It will help Driving Park and the surrounding neighborhoods reconnect with their rich histories and celebrate residents who made significant impacts on American history.” – Mayor Ginther



For Immediate Release

Tuesday, January 31, 2023

Contact: [Trudy Perkins/Dani Carlson](#)

(202) 224-3978

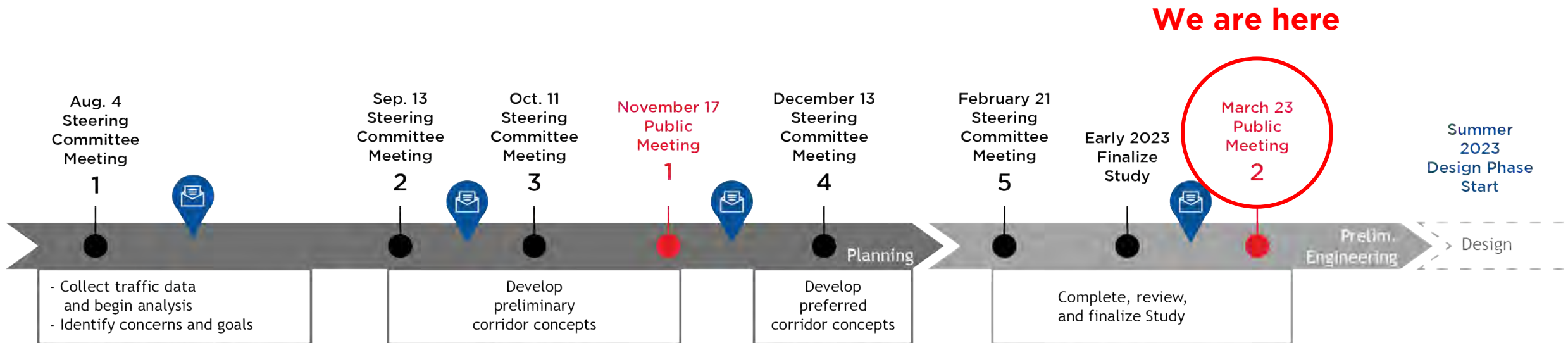
BROWN ANNOUNCES MORE THAN \$15 MILLION FOR STREET SAFETY ACROSS OHIO

WASHINGTON, D.C. – Today, U.S. Senator Sherrod Brown (D-OH), chairman of the Senate Banking, Housing, and Urban Affairs Committee, announced that the U.S. Department of Transportation (DOT) awarded 14 Safer Streets for All (SS4A) grants totaling \$15,414,000 to 14 communities across Ohio. This funding was included in the *Infrastructure Investment and Jobs Act*, also known as the Bipartisan Infrastructure Law – legislation that [Brown helped write and pass](#) – and will support the development of safety action plans and safety improvements.

“With this investment from the Bipartisan Infrastructure Law, we are working to make Ohio roads safer for the thousands of Ohioans who travel on them each day,” said Brown. “This funding will put in place safety plans that will help to prevent serious accidents and keep pedestrians, bicyclists and drivers safe.”

“I am grateful to the Biden administration, Transportation Secretary Buttigieg, Senator Brown and Congresswoman Beatty and so many of our residents for their support of the Livingston Avenue project,” said Mayor Andrew J. Ginther. “This investment will significantly improve the safety and accessibility of the corridor, businesses and other community assets. It will help Driving Park and the surrounding neighborhoods reconnect with their rich histories and celebrate residents who made significant impacts on American history.”

Next Steps: Study Phase



Next Steps: Study Phase → Design Phase

- Study recommendations/concepts will be implemented in the design phase
 - Additional data collection and analysis may be necessary
- Design Phase will coordinate with the following entities during design:
 - COTA
 - City of Columbus – DPS, CRPD, DOSD, DOW, DOP, DOT, Fire Dept., etc.
 - Private Utilities
 - ODOT
- Continued public involvement at key points during design phase

Next Steps: Anticipated Project Schedule

The following anticipated schedule is set to meet requirements of the Safe Streets for All grant agreement.

Study: Complete Spring 2023

Design: Summer 2023 to End 2025

Right of Way Acquisition: Spring 2025 to Spring 2026

Utility Relocation: Spring 2025 to Spring 2026

Construction: Summer 2026 to End 2028

How to Submit Comments

At the Meeting

Comment form at comment table

After the Meeting

Online (QR to the right)

Project Manager, Eric Lambert

- Email: SELambert@columbus.gov
- Mail a comment form to:

Eric Lambert

111 N. Front Street, 6th Floor

Columbus, OH 43215



www.tinyurl.com/LivingstonSurvey

2

Please submit comments by April 23!

Future Project Updates

Project website -

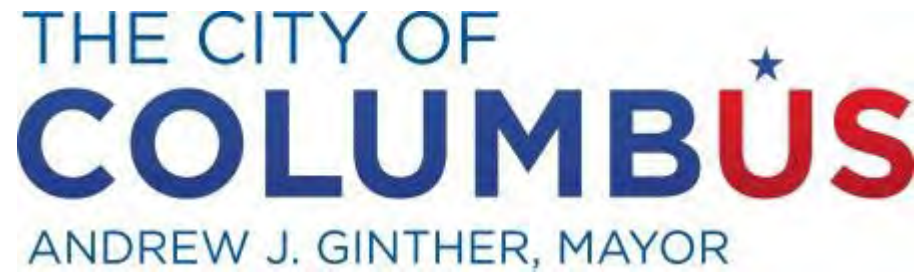
<https://www.columbus.gov/LivingstonAvenuePedestrianSafetyStudy/>

- General Project Information
- Updates on Public Involvement Events
- Newsletters
- Public Meeting Materials
- General Public Comment Form

Public Comment Form:

<https://docs.google.com/forms/d/e/1FAIpQLSdmld62mfTY0UD7qEEV8h7J423lg6Kq5ufoqCiG3a8pr9gb8A/viewform>

Thank you!



1. Why is this study needed?

The Livingston Avenue corridor is identified on the Vision Zero Columbus High Injury Network because it has a higher density of fatal and serious injury crashes that present safety issues for pedestrians, cyclists, transit users and drivers. Compared to the rest of Columbus, fatal crashes are six times more likely to occur along Livingston Avenue. Many individual improvements have been made to the corridor, however a comprehensive study for this area has yet to be done. The City is working to address these issues along the corridor, with the Livingston Avenue Improvements Study.

2. What is the purpose of the study?



The study is focused on Livingston Avenue from 18th Street to Nelson Road. The goal of the study is to identify improvements to the corridor to promote safety for all roadway users and reduce vehicle speeds and severe crashes. The study process will identify issues, conduct analysis, receive and record input, and narrow down solutions. The process will result in a report that will compile all information in a reader-friendly document and will be shared with the public. The results of the study are then used as a roadmap/reference for future project(s).

3. What's been done so far?

The city and consultant team have been working with a Steering Committee, comprised of neighborhood residents and business, faith, and community organization representatives, to develop recommendations to improve safety on Livingston Avenue. Based on the feedback received at steering committee meetings 1-3, as well as technical data collected from the team, the project team developed preliminary corridor recommendations to improve Livingston Avenue. These recommendations were presented at the first public meeting so that attendees could provide feedback. The general concept presented at the first public meeting was a road diet (reduced to 2 through lanes with center or left turn lane/median) with a raised bike lane on either side, grass tree lawn area, and sidewalk improvements.

Top themes from comments received at the first public meeting include:

- Many commenters thought the concept will improve safety
- Positive feedback on dedicated, raised bike lane
- Positive feedback on medians, but concerns about cutting off access to certain side streets on Livingston
- Some concern about reduced travel lanes, but overall positive feedback on road diet concept
- Mixed comments/feedback on removal of on-street parking

4. What is the purpose of the second public meeting?

The purpose of the second public meeting is to present the recommendations and findings made during the study and planning process. The team will also share the next steps for the overall project.

5. What recommendations will be included in the study?

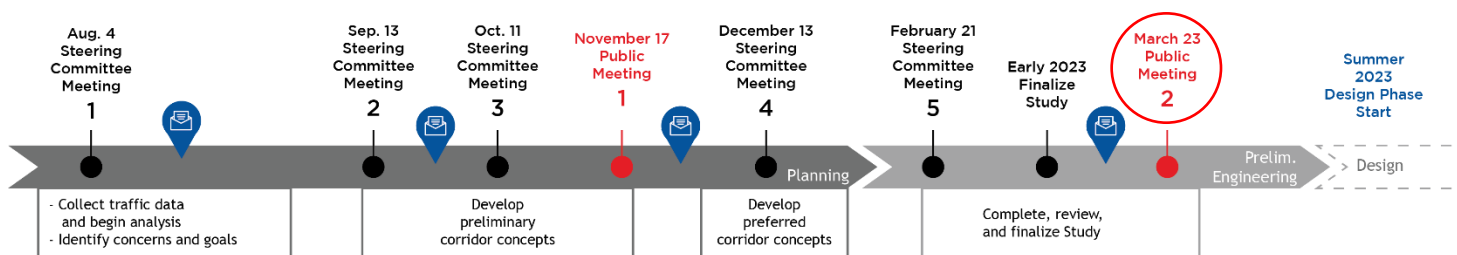
Considering public and steering committee feedback, the team will include the following recommendations for further development in the design phase:

- Road Diet. The road diet will reduce the number of lanes from two through lanes in each direction to one through lane in each direction. The two lanes will then be reallocated to other uses, such as bike facilities, medians, turn lanes, or parking. The typical sections on the reverse page show the variety of layouts that may be utilized along Livingston Avenue.
- Transitions at project limits (to tie into existing bike and pedestrian infrastructure for better connectivity)
- Pedestrian accommodations
- Parking considerations
- Other recommendations such as bus stops, cross-street realignments, and consideration for opportunities to include community identity or beautification elements

All recommendations made in the study will be considered in the design phase. Traffic analysis is ongoing.

6. What is the anticipated schedule for the study?

Below is the study timeline, with design beginning in summer 2023:



7. What is the difference between the study phase and the design phase?

During the study phase, corridor issues and project goals were identified, traffic analysis was conducted, and input was received to identify recommended concepts to improve safe travel along the corridor. This information will be documented in the Livingston Avenue Improvements Study report. The study will be used as a road map/reference for the project as it moves into the design phase to be implemented. The design phase will begin to work through the recommendation details to prepare for construction of the improvements.

8. Will the public be updated on progress during the design phase?

Yes, public involvement and opportunities to comment will continue into the design phase. Please let us know on the comment form how the team can best provide project updates in the future.

9. When will this get built?

It is anticipated that construction will begin Summer 2026 and end Winter 2028. A general schedule from study completion (Spring 2023) to construction is below:

- **Livingston Avenue Improvements Study:** Complete Spring 2023
- **Design:** Summer 2023 to End 2025
- **R/W Acquisition:** Spring 2025 to Spring 2026
- **Utility Relocation:** Spring 2025 to Spring 2026
- **Construction:** Summer 2026 to End 2028

10. How can I submit comments?

At the meeting, you can fill out a comment form at the comment table and give it to a team member or place it in the “Comments” box. If you’d like to submit a comment after the meeting, you can do so by the following methods:

Online comment form:

<https://tinyurl.com/LivingstonSurvey2>

Email Eric: SELambert@columbus.gov

Mail a comment form to:

Eric Lambert
111 N. Front Street, 6th Floor
Columbus, OH 43215



Comments will be accepted at any time; however, comments submitted by April 23, 2023, will be documented in the study and provided a response.

Livingston Avenue Improvements Study, Public Meeting #1 - Public Comments and Responses

Topic/Theme	Comment/Question (Summarized)	Project Team Response
Safety	I like the concept and think that it will slow traffic/improve safety.	Thank you for your comment. We also believe the concept will improve safety and slow speeds. The Federal Highway Administration (FHWA) has done numerous studies on the benefits of road diets. A road diet is a roadway technique that reduces the number of vehicle lanes for space to be reallocated for other uses. Narrowing a roadway from 4 lanes to 3 lanes can provide a 19-47% reduction in total crashes (www.highways.dot.gov/safety/other/road-diets). The reduction in lanes provides room for additional safety improvements such as medians and pedestrian refuge islands, crosswalk enhancements, bicycle lanes, and access management. Incorporating pedestrian and bicycle facilities can increase drivers' recognition of these users. These visible cues increase driver awareness and encourage motorists to slow down and pay attention.
	The speed should be lowered on Livingston.	Thank you for your comment. Studies have shown that most drivers tend to drive at a speed with which they are comfortable, so raising or lowering the speed limits may not have a significant effect on actual speed along the roadway. Additionally, Livingston Avenue is also U.S. Route 33, which limits the city's ability to change the speed limit. However, changing the environment of a roadway by installing traffic calming measures, such as a road diet, can reduce speed and change how drivers behave in the roadway. Environmental adjustments, such as narrowing roadways or installing physical barriers, force motorists to pay attention to their surroundings and alter their driving behaviors, resulting in lower speeds and safer driving. Narrowing a roadway from 4 lanes to 3 lanes can provide a 19-47% reduction in total crashes (www.highways.dot.gov/safety/other/road-diets).
	How will the area around Rhoads / Fire Station / New Development work? Lots of pedestrians cross at that area.	Thank you for your comment. The recommended road diet will help improve safety and comfort for pedestrians and cyclists by reducing overall pedestrian crossing distance and reducing vehicle speeds. The team is currently studying different options at the Rhoads intersection to try to satisfy the various needs of the area (fire station and community center access, vehicle safety, and pedestrian safety) and make the area safer for all modes of transportation. Coordination with surrounding property owners, including the fire station and other agencies, will occur prior to finalizing the roadway configuration. This area will continue to be a focus as the project moves into detailed design.
	Is there any way to reduce heavy vehicle traffic on side streets?	Thank you for your comment. The study recommends the strategic installation of medians along Livingston Avenue to increase safety, provide access management, and allow opportunities for beautification. Medians can restrict vehicular access to residential roadways and redirect traffic to other nearby streets that are more equipped for heavy traffic volumes. This can help reduce heavy vehicle traffic on side streets.

Livingston Avenue Improvements Study, Public Meeting #1 - Public Comments and Responses

Topic/Theme	Comment/Question (Summarized)	Project Team Response
Bike Lanes	I like that there is a dedicated bike lane.	Thank you for your comment. Installing a dedicated bike lane along Livingston Avenue will provide residents, commuters, and visitors an alternate mode of transportation in the corridor. Protected bike lanes help eliminate perceived risk and fear of collisions, reduce the risk of crashing into car doors, and add a level of predictability making streets safer for everyone. Installing a dedicated bike lane for cyclists can reduce total crashes up to 30% on urban 2-lane undivided collectors and local roads. (https://highways.dot.gov/sites/fhwa.dot.gov/files/2022-08/PSC_New_Bicycle%20Lanes.pdf)
	I like that the bike lane is raised.	Thank you for your comment. The steering committee preferred the raised bike lane versus the street level bike lane because it provides an additional layer of protection from vehicles. This makes the raised bike lane more attractive to a wider range of bicyclists at all levels and abilities than a lane that is shared or at the same level as motorized vehicles. Raising the bikeway above the street grade can further discourage motorists from encroaching into the raised bike lane.
	I don't like that the bike lane is raised. It makes cyclist and drivers less aware of their surroundings.	Thank you for your comment. A variety of different bike facilities were considered during the planning process. The raised bike lane was chosen as the recommended bike facility because of the additional safety benefits of a dedicated, separated bikeway. Additionally, the corridor has limited width, and raised bike lanes are efficient with space in tight corridors.
Medians	I like the added greenspace/medians in the concept.	Thank you for your comment. There are many benefits to installing raised medians. The FHWA shows that raised medians provide a refuge space for crossing pedestrians (resulting in a 46% reduction in pedestrian crashes), can reduce motor vehicle crashes by 15%, reduce vehicle speeds on the roadway, and provide a space for vegetation to help "soften" the feel of a corridor. Medians also provide access management to nearby side streets.
	The medians are too long and cut off access to neighborhoods/side streets.	Thank you for your comment. Medians were included in the concept presented at the first public meeting so that feedback could be gathered from the public. Additional turn lanes and access points ("breaks" in the medians) are anticipated to manage access properly. The design phase will be responsible for determining the best use of medians to balance safety benefits while managing access.

Livingston Avenue Improvements Study, Public Meeting #1 - Public Comments and Responses

Topic/Theme	Comment/Question (Summarized)	Project Team Response
Parking	I like that the on-street parking was removed on Livingston.	Thank you for your comment. Parking usage data was gathered by the project team on a typical weekday during the morning, midday, and evening as well as on a Saturday in the morning, evening, and midday. The data showed that parking on Livingston Avenue is relatively light. Only a few blocks were more than 30% filled. The blocks that had above 30% utilization were: along the north side of Livingston between Gilbert St. and S 22 nd St., along the north side of Livingston between S Champion Ave. and Oakwood Ave., and along the north side of Livingston between Lockbourne Rd. and Miller Ave. The project team understands that parking can provide challenges for some roadway users yet is important for others. Due to the limited space within right-of-way, parking cannot be implemented where a turning lane is required or where a median is needed/desired. A parking option is included in the recommendations for areas where parking may be warranted (now or in the future) while not conflicting with necessary turning lanes. The design process will be responsible for properly employing the recommendations from the study and deciding where parking may be implemented.
	I dislike that the on-street parking was removed on Livingston.	Thank you for your comment. Please refer to the above response.
Road Diet	I like that the traffic lanes are being reduced.	Thank you for your comment. The reduction in travel lanes is also referred to as a “road diet.” Studies by the FHWA show that road diets can reduce crashes by 19 to 47% and reduce vehicle speeds. A road diet on Livingston Avenue will improve safety for all roadway users, roadway operations, and better meet the needs of multiple modes of transportation and the community as a whole.
	I dislike that the traffic lanes are being reduced. <ul style="list-style-type: none"> - Increased travel times - Stuck behind busses 	Thank you for your comment. Currently, all the signalized intersections on Livingston Avenue operate at an acceptable Level of Service. Traffic models show that with the implementation of a road diet, all signalized intersections remain in the acceptable range. Although the traffic lanes are being reduced, the installation of additional dedicated left turn lanes will improve operations on Livingston Avenue by removing cars slowing down to make left turns. Some users may also elect to reroute their trip entirely around Livingston Avenue due to the slower speed, which will decrease traffic on Livingston Avenue, allowing it to flow more smoothly. With the additional roadway available for the installation of bike lanes, other users may bike instead of drive on Livingston Avenue, further decreasing traffic on the roadway. Coordination with COTA is ongoing as the project moves into the design phase.

Livingston Avenue Improvements Study, Public Meeting #1 - Public Comments and Responses

Topic/Theme	Comment/Question (Summarized)	Project Team Response
Beautification / Aesthetics	I think there should be more beautification in the concept: Installation of stone placards, landscape lighting, other aesthetics.	Thank you for your comment. The Steering Committee and comments received from the public indicate that there is a high level of interest in creating a community identity and beautification/aesthetics as part of the Livingston Avenue improvements. At this stage in the study, specific aesthetic treatments and options have not been identified. Details on beautification/aesthetic elements will be determined during the design phase. Public involvement will be ongoing.
	I think the concept is aesthetically pleasing.	Thank you for your comment.
Project & Schedule	I like that there's something being done about the safety along Livingston Avenue.	Thank you for your comment. Livingston Avenue is on the City's High Injury Network, which means that it has a higher proportion of fatal, serious injury, and/or vulnerable user crashes. Vulnerable road users include pedestrians, bicyclists, or motorcyclists - individuals who are inherently more at risk of injury when involved in any traffic crash. Vision Zero Columbus is focused on ending crash-related fatalities and serious injuries on our streets, while increasing safe, healthy, and equitable mobility for all. We're excited to take this first step in much needed improvements to Livingston Avenue.
	I wish the project would be done sooner than the expected schedule.	Thank you for your comment. We also wish this project could be accomplished sooner. The project funding includes a recently awarded \$12 million from the U.S. DOT, which requires the project to fully comply with applicable federal laws and regulations. The efforts required to fully comply with these laws can add time to a project schedule when compared to the same project that is funded completely with local funds. It is the city's goal to complete this project as quickly as possible.
	The project area is too small. It should extend along some side streets.	Thank you for your comment. The project limits are along Livingston Avenue from 18 th Street to Nelson Road. However, we anticipate that the improvements of the project will extend where needed (north, south, east, or west) to properly tie into the rest of the corridor and transition properly within the surrounding neighborhoods.
	Were any lessons learned from the project between College and James that will be incorporated into this project?	Thank you for your question. The city employs a continuous improvement process to improve the quality of each project it delivers. While there are similarities between projects, each project presents its own unique challenges.
	The project cost is too high.	Thank you for your comment. The city believes the Livingston Corridor is very worthy of this investment, especially considering the proposed safety benefits for the traveling public. Estimated project costs were developed based on actual costs of similar projects. The city was successful in securing a \$12 million grant from USDOT to help leverage local funds being provided for this project.

Livingston Avenue Improvements Study, Public Meeting #1 - Public Comments and Responses

Topic/Theme	Comment/Question (Summarized)	Project Team Response
Crosswalks	I like the additional crosswalks.	Thank you for your comment. Studies from the FHWA show that high-visibility crosswalks can reduce pedestrian injury crashes up to 40% (https://highways.dot.gov/safety/proven-safety-countermeasures/crosswalk-visibility-enhancements). Safe and frequent crosswalks will support pedestrian movement on Livingston Avenue. Crosswalks have been recommended in areas where pedestrian traffic is anticipated and encouraged.
	<p>I don't think enough crosswalks have been added.</p> <ul style="list-style-type: none"> - More between Linwood and Lockbourne 	<p>Thank you for your comment. The study recommends crosswalks on Livingston Avenue at existing signalized intersections and areas where pedestrian traffic is anticipated. Rectangular rapid flashing beacons (RRFB) placed recently at 22nd Street, Oakwood Avenue, and Lilley Avenue will remain. Additionally, a RRFB will be added to the intersection of Livingston Avenue and Bulen Avenue. Pedestrians are encouraged to cross at these signalized crosswalks because it is safer than crossing in an area without a marked crosswalk and drivers will expect pedestrians in these areas. Studies from the FHWA show that high-visibility crosswalks can reduce pedestrian injury crashed up to 40% (https://highways.dot.gov/safety/proven-safety-countermeasures/crosswalk-visibility-enhancements). The recommended road diet will also significantly increase the safety for pedestrians crossing Livingston Avenue by reducing the crossing distance pedestrians will travel and potential impact points. The sections of Livingston Avenue with a median will allow for additional safety benefits since the median provides a refuge space for crossing pedestrians. The study also undertook efforts to identify which areas were likely to need pedestrian facilities, such as the Pedestrian Generator Map, to identify crossing points and areas of high pedestrian traffic. Intersections such as Carpenter Street and Rhoads Avenue have been identified as such areas and were recommended to receive additional pedestrian safety countermeasures.</p>
Busses	How will the busses navigate with the bike lane? The intersections seem too tight.	Thank you for your comment. A preliminary layout of the concept on Livingston Avenue shows that there is enough room for buses to travel through Livingston Avenue without issue. As the project moves into detailed design, coordination with COTA will ensure that the project will be designed so that buses will be able to travel and provide efficient service throughout the study area.
	Bus routes will be delayed because of the reduction in travel lanes.	Thank you for your comment. We do not anticipate a significant change in travel time for transit. As the project moves into detailed design, coordination with COTA will ensure that the project will be designed so that buses will be able to travel and provide efficient service throughout the study area.

Livingston Avenue Improvements Study, Public Meeting #1 - Public Comments and Responses

Topic/Theme	Comment/Question (Summarized)	Project Team Response
Economic Activity	We need retail, restaurants, grocery options on Livingston.	Thank you for your comment. The scope of this project is limited to improvements within the right-of-way. The FHWA released information about the economic impacts of a road diet, such as proposed in the Livingston Avenue Improvements Study. The research found that replacing vehicle travel lanes with pedestrian and bike facilities make the corridor a more attractive area and thus more appealing to business and development interest. From the FHWA Road Diet Resources, <i>Road Diets' Economic Impacts</i> : "In 2008, the City of Indianapolis, IN, used Road Diets to complete the 8-mile-long Indianapolis Cultural Trail that encourages biking and walking along the cultural districts, neighborhoods, and the city's greenway system. The redesign brought more people on foot and vitalized the area. Over \$300 million of new development was constructed along the route between 2008 and 2012."
	I think the improvements will increase business activity in the area.	Thank you for your comment. The FHWA released information about the economic impacts of a road diet and found that for many local businesses, a road diet can improve economic vitality by changing the corridor "from a place that people 'drive-through' to one that they 'drive-to'." A case study from Charlotte, NC showed that after the implementation of a road diet there was a \$43 million increase in the non-residential tax value of properties fronting the East Boulevard Road Diet.
Misc.	Don't do what was done on Parsons.	Thank you for your comment. The city aims to genuinely evaluate and learn lessons from every project. If you're like to discuss further please contact the project manager, Eric Lambert at SELambert@columbus.gov .
	I don't like the vacant buildings along Livingston.	Thank you for your comment. Although this is out of the scope of the project, the FHWA has released information about the economic impacts of a road diet and found that for many local businesses, a road diet can improve economic vitality. The installation of a road diet on Livingston Avenue may be attractive to business owners and developers that would seek to revitalize vacant buildings.
	There is a curb issue on Gilbert Street	Thank you for your comment. This is out of the scope of this study; however, a request can be submitted to report the curb issue to the city via our 311 customer service center online at www.columbus.gov/311 , via email at 311@columbus.gov , or by phone at (614) 645-3111.
	I like the improved sidewalks.	Thank you for your comment. Pedestrian infrastructure is key to mobility for a variety of users. Adequate sidewalks are shown to improve pedestrian safety, improve the health of nearby residents, increase property values, and generally improve quality of life. According to the FHWA, roadways without sidewalks are more than twice as likely to have pedestrian crashes as sites with sidewalks on both sides of the street.
	Will the utilities be put underground? I would like to see that. It makes the area look much nicer.	Thank you for your comment. At this point in the project, there is no plan to move the overhead utilities underground. This is due to cost factors and existence of current underground utilities along Livingston.



Highlights

- City of Columbus completing study to make improvements to Livingston Avenue from 18th St. to Nelson Rd.
- Fifth Steering Committee meeting held February 21st
- Public meeting #2 is March 23, 2023
- Study phase expected to be complete Spring 2023
- Design phase to start Summer 2023

Questions or Comments?

Contact:

Eric Lambert

City of Columbus, Project Manager
614-645-2732

SELambert@columbus.gov

For more information:

[www.columbus.gov/
LivingstonAvenuePedestrianSafetyStudy](http://www.columbus.gov/LivingstonAvenuePedestrianSafetyStudy)



This newsletter provides information and updates on the Livingston Avenue Improvements Study.

Project Background

Livingston Avenue is on the Vision Zero Columbus High Injury Network. The corridor has a higher density of fatal and serious injury crashes that threaten the safety of pedestrians, cyclists, transit users and drivers. The city is addressing this issue along the corridor, with the Livingston Avenue Improvements Study focused on the section from 18th Street to Nelson Road to promote safety for all roadway users and reduce vehicle speeds and severe crashes.

Public Meeting #1 Recap



Public Meeting #1 - November 17, 2022

- Shared preliminary corridor concepts and requested feedback from attendees

Top Themes from Comments

- Many comments thought the concept will improve safety
- Positive feedback on dedicated, raised bike lane
- Positive feedback on medians, but concerns about cutting off access to certain side streets on Livingston
- Some concern about reduced travel lanes, but overall positive feedback on road diet concept
- Mixed comments/feedback on removal of on-street parking



Full comments summary and responses available on project website (left).

Upcoming Public Meeting Open House #2

The purpose of the second public meeting is to present the recommended corridor concepts and findings made during the study and planning process. The corridor concepts were developed with input from the project Steering Committee, comprised of neighborhood residents and business, faith and community organization representatives.

Since the meeting will be held in an open house style format, you're welcome to join us at any time during the time frames listed to the right. There will be a presentation at 12:30 p.m. and 6 p.m. City staff and the study project team will be on hand to help answer questions.

To receive a copy of materials presented at the public meeting, please contact Eric Lambert at the contact information above to provide your name and address.

All are welcome!

March 23

**Driving Park
Community Center**

12 p.m. - 2 p.m.

Presentation at 12:30 p.m.

&

5:30 p.m. - 7:30 p.m.

Presentation at 6 p.m.

Next Steps



Moving into the Design Phase

During the study phase, corridor issues and project goals were identified, traffic analysis was conducted, and input was received to identify recommended concepts to improve safe travel along the corridor. This information will be documented in the Livingston Avenue Improvements Study. The study will be used as a road map/reference for the project as it moves into the design phase to be implemented. The design phase will begin to work through the recommendation details to prepare for construction of the improvements. Public involvement and opportunities to comment will continue into the design phase.

Steering Committee Members

Michael Aaron

Rickenbacker Woods Community Foundation

Pastor Jonathan Bradford

Bethany Baptist Church

Danielle Dillard

Livingston Avenue Area Commission

David Gray

Livingston Avenue Area Commission

Stacey Green

Driving Park Civic Association

Rita Hallaveld

Resident & Cyclist

Molly Hogue

Resident

George Holliman

Livingston Avenue Area Commission

Rachel Robinson

Southern Orchards Civic Association

Jerry Saunders

Africentric Personal Development Shop

Erin Synk

Columbus South Side Area Commission

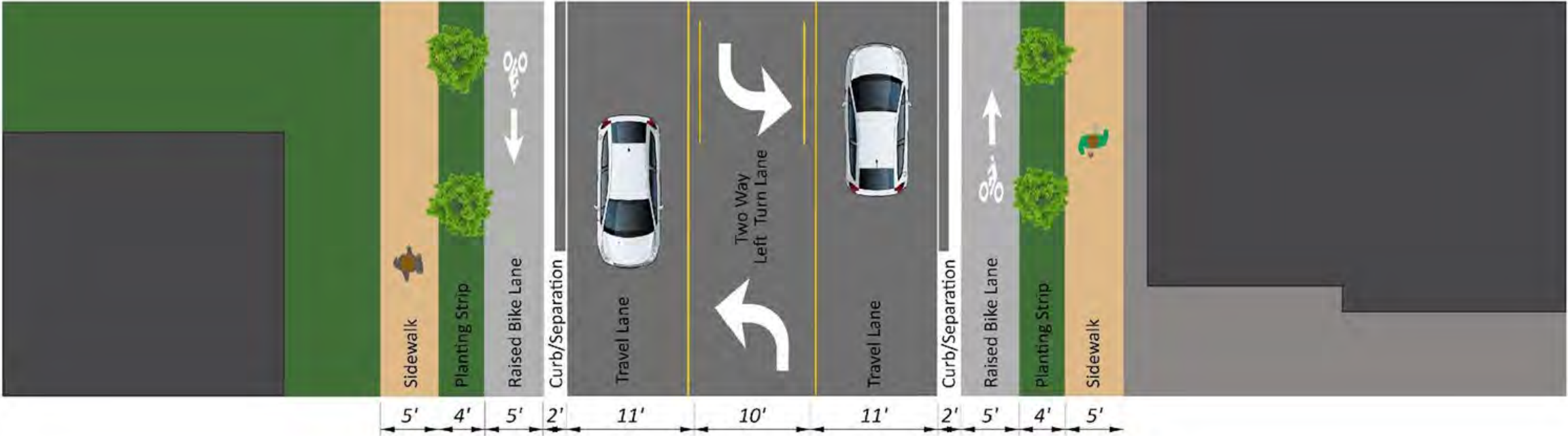
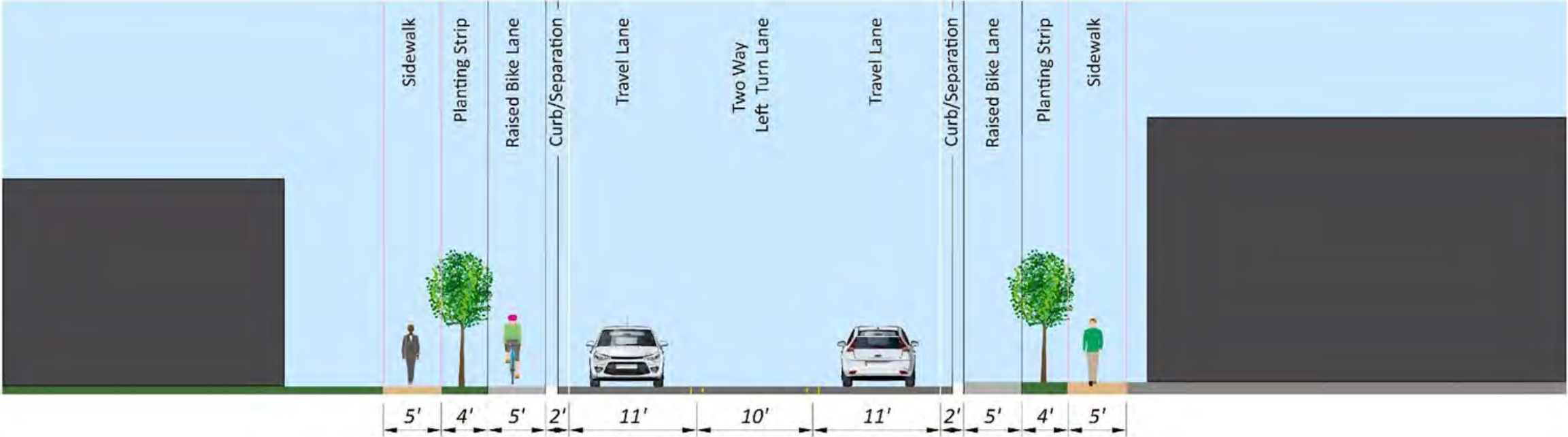
Ralph Walker

Livingston Ave. Columbus OH Safety Group

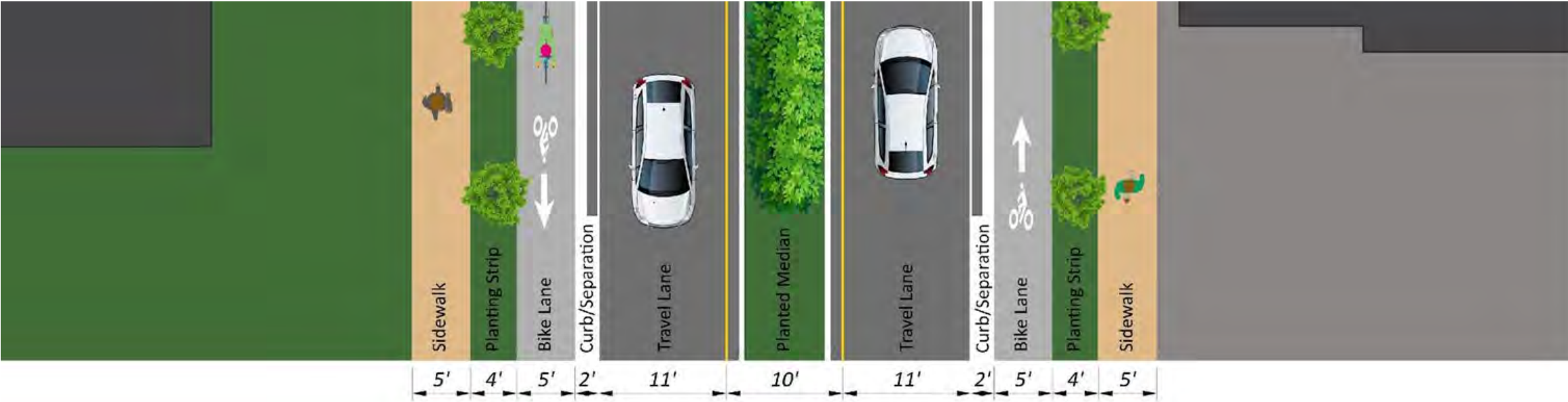
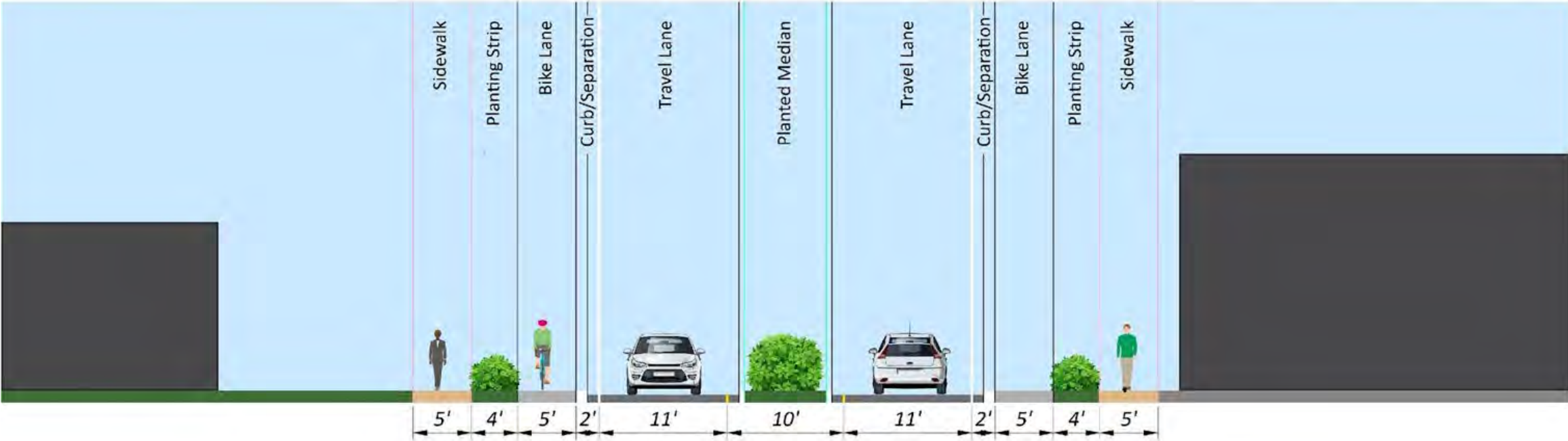
Sherman Willis

Community Business Owner

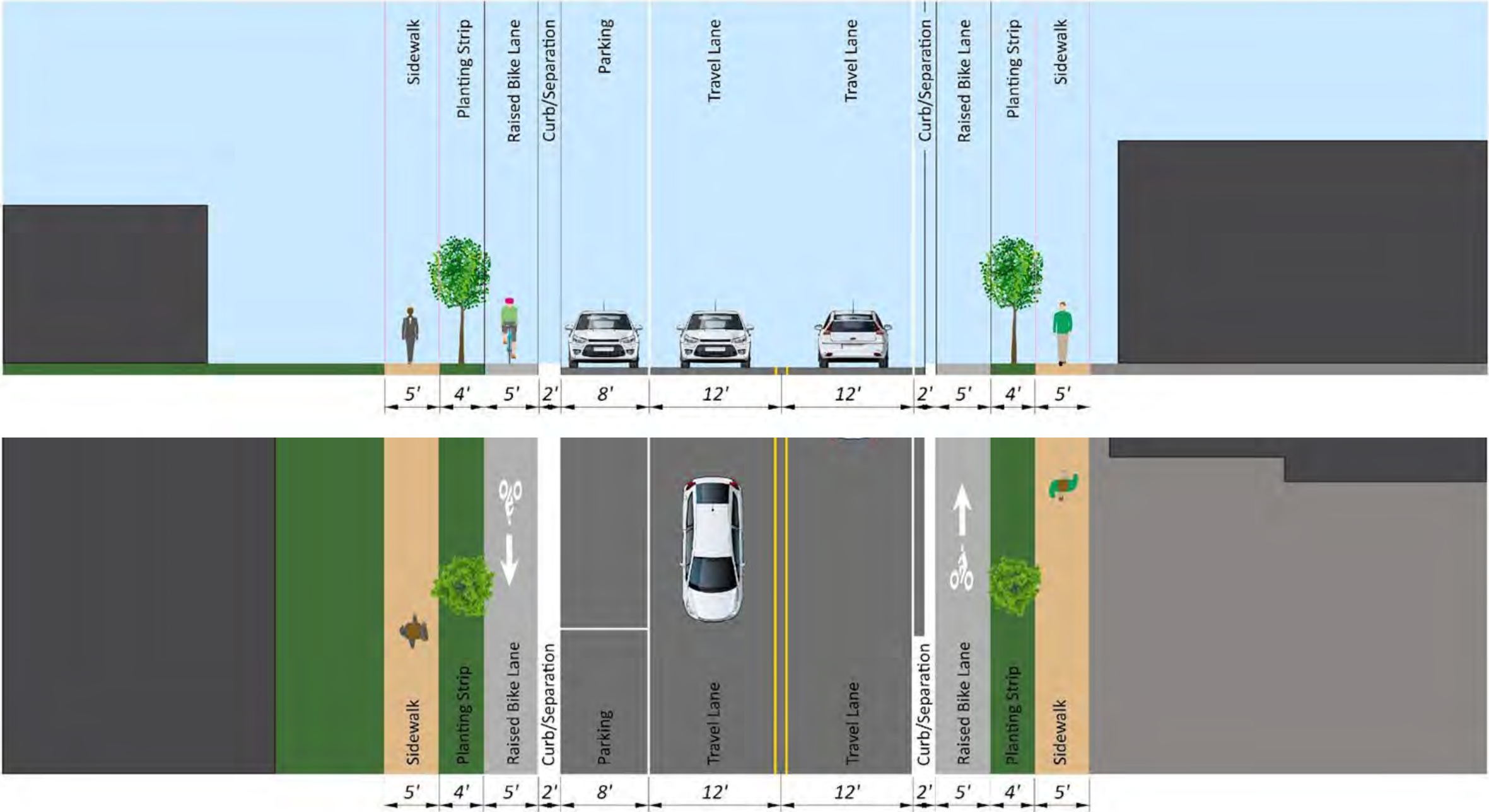
Study Recommendation shown with Two Way Left Turn Lane



Study Recommendation shown with Planted Median



Study Recommendation shown with Parking













Public Meeting #2 Survey & Comment Form

Comments will be accepted at any time; however, comments submitted by **April 23, 2023** will be documented in the study and provided a response.

Online Comment Form: www.tinyurl.com/LivingstonSurvey2 —————→



Contact Information

Name: _____

Email (for future updates on the project): _____

Survey

1. How did you hear about the public meeting? (Select all that apply)

- | | | |
|--|--|---------------------------------------|
| <input type="checkbox"/> City website | <input type="checkbox"/> TV or radio | <input type="checkbox"/> Other: _____ |
| <input type="checkbox"/> Mailed postcard | <input type="checkbox"/> Church | _____ |
| <input type="checkbox"/> Social media | <input type="checkbox"/> Word-of-mouth | _____ |

2. What are the most effective communication methods to provide project updates or invitations for future project meetings? (Select all that apply)

- | | | |
|--|---|---------------------------------------|
| <input type="checkbox"/> City website | <input type="checkbox"/> Email list | <input type="checkbox"/> Other: _____ |
| <input type="checkbox"/> Mailed postcard | <input type="checkbox"/> Area Commission meetings | _____ |
| <input type="checkbox"/> Social media | <input type="checkbox"/> Periodic newsletter | _____ |

3. What is your relationship to Livingston Avenue? (Select all that apply)

- | | | |
|--|-----------------------------------|---------------------------------------|
| <input type="checkbox"/> Area resident | <input type="checkbox"/> Commuter | <input type="checkbox"/> Other: _____ |
| <input type="checkbox"/> Area business owner or employee | | _____ |

4. What is your primary mode of transportation during a typical work week? (Select all that apply)

- | | | |
|---|---|---------------------------------------|
| <input type="checkbox"/> Personal vehicle | <input type="checkbox"/> Ride share | <input type="checkbox"/> Other: _____ |
| <input type="checkbox"/> Carpool | <input type="checkbox"/> Public transit | _____ |
| <input type="checkbox"/> Bicycle | <input type="checkbox"/> Walk | _____ |

5. How often, on average, do you typically bike or walk on Livingston Avenue? (Select one)

- | | | |
|---------------------------------|---|---------------------------------------|
| <input type="checkbox"/> Daily | <input type="checkbox"/> Monthly | <input type="checkbox"/> Almost never |
| <input type="checkbox"/> Weekly | <input type="checkbox"/> A few times a year | |

6. How often, on average, do you typically ride the bus or use public transportation? (Select one)

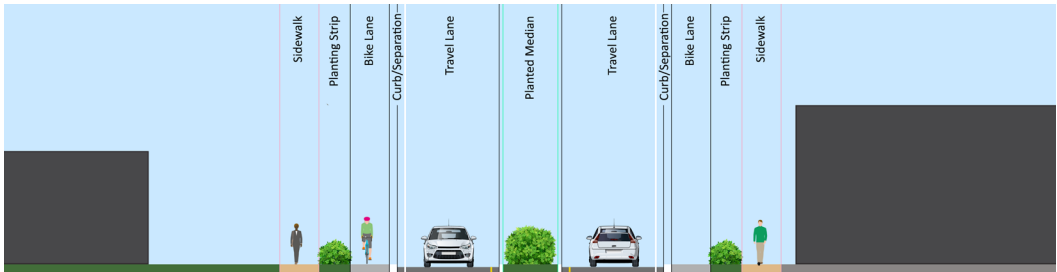
- | | | |
|---------------------------------|---|---------------------------------------|
| <input type="checkbox"/> Daily | <input type="checkbox"/> Monthly | <input type="checkbox"/> Almost never |
| <input type="checkbox"/> Weekly | <input type="checkbox"/> A few times a year | |

Proposed Improvements

Please rate the following options 1-5 with 1 being least interested in this improvement and 5 being the most interested (please circle only one).

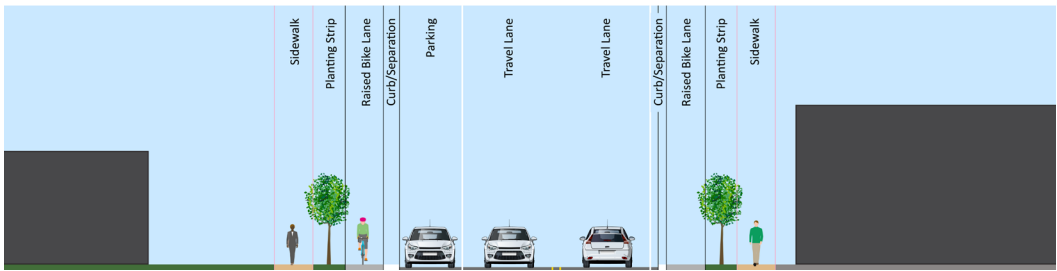
The following images represent a typical section on Livingston Avenue with a road diet (reducing travel lanes to one in each direction) and raised bike lanes. Each image represents a different use for the remaining roadway space, including a planted median and on-street parking.

Planted Median



Least interested in this improvement 1 2 3 4 5 Most interested in this improvement

On-Street Parking



Least interested in this improvement 1 2 3 4 5 Most interested in this improvement

Comments or Feedback:

This comment form may be submitted to Eric Lambert, the Project Manager, by the following methods:

In-person at the meeting: Drop into the comment box
Email to: SELambert@columbus.gov
Mail to: Eric Lambert
111 N. Front Street
Columbus, OH 43215

Online Comment Form

