



BIKE LANE



A bicycle lane is a part of the road designated for bicyclists to use. This is done with pavement markings—usually solid white lines, with a white bicycle symbol. Often signs along the roadway alert motorists and bicyclists to these lanes. Some bike lanes may have green pavement to make it easier for cars and bicyclists to see them.

A bike lane generally has one-way traffic, traveling in the same direction as the adjacent vehicles. As a bike lane approaches an intersection or bus stop, the white lines become dotted to indicate that turning motorists should merge into the bike lane. Drivers should watch for bicyclists when they see a bike lane road marking and, when turning, yield to bikes in the bike lane before completing the turn.

BIKE LANES

BIKE ROUTES

Another way you can ride your bicycle around the City is on a bike route. A bike route is a road or a group of road segments designated with “bike route” signs. The signs guide bicyclists to a specific destination and may include directional and distance information. Motorists will know a road is a part of a bike route because of the green bike route signs. So keep an eye out for bicycles and always share the road.

BIKE TRAILS & SHARED-USE PATHS

A bicycle trail, also called a shared-use path because both bicyclists and pedestrians use it, is a bikeway separated from vehicle traffic by an open space or a barrier. There are two main types—a side path, close to the roadway—like on Dublin Road near West Fifth Avenue or a path in a park like setting or through woods—like the Olentangy Greenway Trail. Pedestrians, inline-skaters, wheelchair users, joggers and others on non-motorized vehicles also may use them. Most shared-use paths allow two-way travel.

In Columbus you can ride your bicycle on the street, in a bike lane or lane painted with a “sharrow,” on a bike trail or shared-use path or along a bike route. Although the names sound similar, bicyclists use them differently.

Where can you ride your bicycle?

GETTING STARTED

ALONG THE WAY
THROUGHOUT THE CITY OF COLUMBUS
THERE ARE MANY SIGNS TO DIRECT
BICYCLISTS ALONG PATHS AND ROADS.

Mayor Michael B. Coleman created **Share the Road** to make sharing Columbus’ roads safer for everyone—motorists and bicyclists alike.

Share the Road Is Part of the City’s Bikeways Plan. It:

- Provides a long-term vision for the development of a citywide bicycle network consisting of off-street trails and on-street bike facilities complemented by education and encouragement programs
- Promotes active, safe and healthy transportation choices
- Makes Columbus a more attractive and bikeable city for residents and visitors

Under Ohio law* and Columbus City Code, adult bicyclists belong on the road, not on the sidewalk. So, motorists and bicyclists must share the road safely. Bicyclists, like motorists, must obey all relevant traffic laws. **Share the Road** will help Columbus residents understand these laws and encourage everyone to respect each other’s right to use the road.

To make bicycling safer and more attractive to everyone, the City is investing in bike facilities. See enclosed for more information about the bike treatments you will see around the City.

* ORC 4511.07 (A)(B)

THE CITY OF
COLUMBUS
MICHAEL B. COLEMAN, MAYOR

**Share
the Road**

Remember, call the City of Columbus Police Dispatch at 614-645-4545 if you see an immediate issue, violation or accident. You can call 24/7/365.



City of Columbus
Division of Mobility Options
90 W. Gay St., Columbus, OH 43215

Phone 645-3111 or 311
Email 311@columbus.gov
Web: sharetheroad.columbus.gov

THE CITY OF
COLUMBUS
MICHAEL B. COLEMAN, MAYOR

DEPARTMENT OF
PUBLIC SERVICE



Sharing the Road in Columbus

A GUIDE TO

Bicycle Markings for Motorists & Bicyclists

The City is investing in bike facilities to increase bicycle use and make bicycling safer. They include a wide range of improvements.

SHARROWS

A shared lane marking, also called a sharrow, reminds motorists that bicyclists are sharing the traffic lane. It also guides bicyclists on where they should ride within the lane.



There are sharrows on roads throughout the City, including on High and Broad Streets, throughout Downtown, on and near the OSU campus including on Neil and Lane Avenues, on the North Side—on Sharon Woods Boulevard, Karl Road, Cleveland Avenue and SR 161, around Clintonville and other locations too.

BIKE BOULEVARDS

A bicycle boulevard is a low-volume, low-speed street optimized for bicycle travel using traffic calming and traffic reduction treatments, signs and pavement markings and intersection crossing treatments. Parts of streets or a series of contiguous street segments also can be a bike boulevard.

The purpose of a bike boulevard is to provide bicyclists, especially those who are not comfortable riding on busy streets, a safer and more relaxing place to ride.

Bicyclists must obey all traffic laws on a bike boulevard. Motor vehicles are permitted on bike boulevards unless otherwise noted.

There are bike boulevards on Milton Avenue in Clintonville, Steele Avenue on the West Side and along the Westgate Connector.



SHOULDER BIKEWAYS

A shoulder bikeway, also called a paved shoulder, is road shoulder that is paved to make it easier for bicyclists to use. It provides space for bicyclists to ride that is separate from motor vehicle traffic.



A paved shoulder is generally wider to accommodate bicycle travel. Commuter and long-distance recreational riders, rather than families with children or more inexperienced riders, are more likely to use a shoulder bikeway.

There is a shoulder bikeway on Urlin Avenue, between Dublin Road and Grandview Avenue.

BUFFERED BIKE LANES

A buffered bike lane is a bike lane with additional space—a buffer—between bicycles and cars. Bicyclists ride in the bike lane and motorists drive to the left of the buffer.



The purpose of a buffered bike lane is to increase safety by providing extra elbow room for bicyclists and making it easier for a motorist to see bicyclists. The buffer is marked with white chevrons to indicate that no vehicles are allowed to travel in the buffered area.

In a buffered bike lane bicyclists must ride with traffic and obey all the rules of the road.

Vehicles are not allowed in the bike lane or buffered area except when entering and exiting the road, or turning.

When crossing a buffered bike lane to turn, use your mirrors and be sure to look over your right shoulder for bicyclists approaching the intersection, yield and let them pass before turning.

There are a few buffered bike lanes in Columbus, including on Roberts Road between Hilliard Rome Road and Westbelt Drive. Tamarack Boulevard on the North Side also has one.

BIKE BOXES

A bike box is a green marked pavement area at a road intersection with a white bicycle symbol inside it. It also includes an adjacent bike lane for bicyclists to use to move to the front of a line of cars at an intersection, and a stop line on the road for motorists to wait behind.

A bike box can go across one lane or multiple lanes of an intersection. It encourages bicyclists to wait between the queue of cars and the crosswalk. A bike box improves visibility and the safety of bicyclists, allows bicyclists to turn left more safely and reduces the likelihood of cars encroaching into the crosswalk.

At an intersection with a bike box, bicyclists can ride into the bike box using the adjacent bike lane and wait for a green traffic signal. Motorists must wait behind the bike box and behind the stop line at a red signal. When the traffic signal turns green, motorists must yield to bicyclists before proceeding or making a turn.



The City has two bike boxes on the north and south sides of the intersection of Milton Avenue and West North Broadway in Clintonville.

